



# HYAMPOM ROAD PROJECT

California Forest Highway 114, Hyampom Road  
Shasta-Trinity National Forest  
Trinity County, California

# UPDATE

November 2007

## Update on the Hyampom Road Reconstruction Project

### Trinity County Board of Supervisor's Public Hearing held in Hyampom

The Trinity County Board of Supervisors (TCBOS) held a special board meeting at the Hyampom Community Center on the evening of October 3<sup>rd</sup> to gather input on the proposed project and to vote on approval of the California Environmental Quality Act (CEQA) Environmental Impact Report (EIR) document for this project. The FHWA made a presentation to address the concerns of the public and the TCBOS. After public comment, the TCBOS approved the EIR and the addendum with the following options/alternatives:

- All segments (2, 4, and 5)
- Daytime roadway closures with occasional night work if required
- Forest Road 10 being designated only as an alternative route and to receive routine maintenance. There will not be an official detour.

### Scope of Project

Due to public concern regarding the scope and impact of this project, along with rising construction costs, the CFLHD has continued to adjust the typical section and alignment of segment 5 to minimize the footprint and environmental impacts of the project while still meeting the project objectives. This has led to a typical section that includes nine foot lanes, reduced shoulder widths, and adjustments to the alignment to limit large cuts and fills. Reductions from the 15% design to the current design (70%) are as follows:

<i>Description</i>	<i>Preliminary (15%)</i>	<i>Intermediate(50%)</i>	<i>Current(70%)</i>
<i>Area of Impact</i>	65 acres	39 acres	27 acres
<i>Excavation</i>	520,000 cy	360,000 cy	136,000 cy
<i>Retaining Walls</i>	7,600 ft	4,800 ft	3,900 ft

### Geotechnical and Erosion Control Concerns

Concern has been raised regarding the stability of the slopes and rockfall in segment 5 both during and after construction. CFLHD has performed an intensive investigation of the existing conditions and has analyzed the stability of the slopes both during and after construction. It is the opinion of CFLHD that the slopes during and after construction will be more stable. Also, additional rockfall width and protection (draped wire mesh) will be included in the project to improve safety.

Sediment concerns have also been raised. This project will be constructed under a National Pollutant Discharge Elimination System (NPDES) permit that is issued and monitored by the regional water quality board. CFLHD will also limit the ground disturbing activities during the wet season (Nov 1 to April 30) as well as require the contractor to monitor turbidity (sediment) in the adjacent streams. Activities will be halted and measures taken if turbidity readings are outside of acceptable levels.

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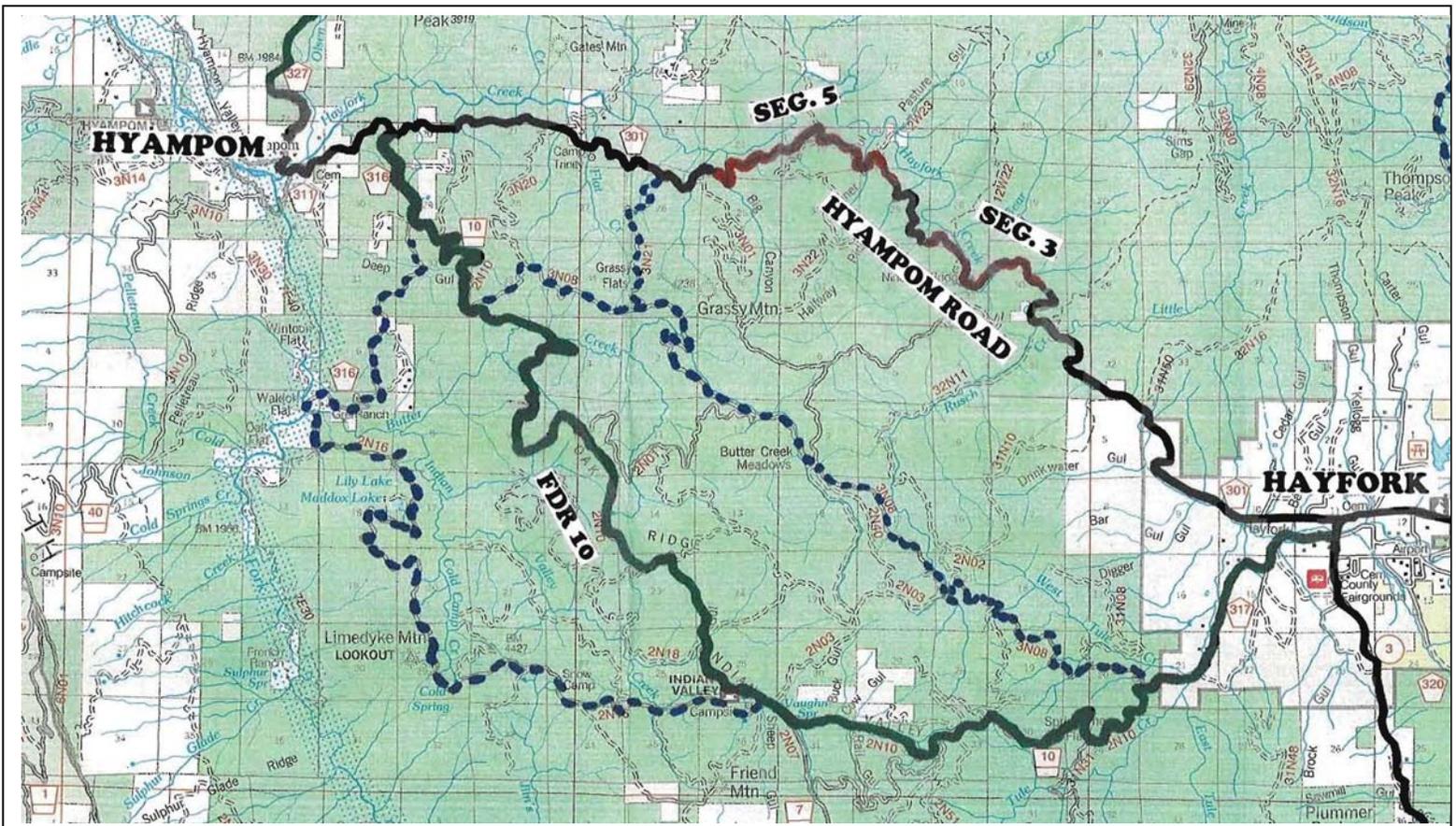
## Access to/from Hyampom during Construction on Alternative Routes

During construction, road users will no doubt be using the other access roads to/from Hyampom more frequently. Of concern was the access to Hayfork. Forest Development Road 10 (FDR 10) is anticipated to be used most frequently as an alternative route to Hayfork. CFLHD presented two options for the use of this roadway during construction, either as a detour or as an alternative route. If FHWA were to designate this road as an official detour, meaning that signing would direct traffic to this road, pilot cars would have to be used due to the limited sight distance, gravel surface, and narrow roadway width along an 11 mile stretch from Indian Valley west to Hyampom Road. This would cause up to 45 to 60 minute delays to traffic waiting for the pilot cars. The other option would be for FHWA to consider this road an alternative route to/from Hyampom, similar to any other route accessing Hyampom. There would not be any officially signed detour and

FHWA would recommend that all traffic wait for openings along Hyampom Road. The TCBOs voted for the second option, using the Forest Road 10 as an alternative route only. The road will receive routine maintenance during construction. Three- to four- hour closures on Hyampom Road will not be allowed until the Forest Road 10 is opened each spring.

## Emergency Opening Procedures

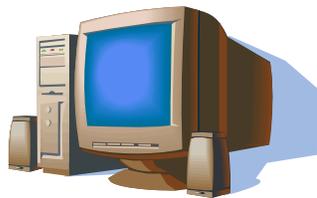
During construction, CFLHD will have a plan in place for communications with all emergency services in order to provide them with the fastest route possible from Hayfork and Hyampom. This will be coordinated through the public information officer, resident engineer, and the contractor's supervisor. CFLHD has performed many projects with similar requirements and is confident in handling these types of situations.



## Road Closures

CFLHD presented a full range of closure schedules to the TCBOS, taking into consideration the length of construction as well as the disruption to the roadway users. Trinity County DOT has coordinated with most, if not all, service providers that use this roadway to determine if a closure schedule could be developed that would minimize disruption and still allow the contractor the maximum allowable work schedule to complete the project in a timely manner. The TCBOS approved the option that included two three- to four-hour closures during the daytime and only occasional 30 minute maximum delays at night. A more detailed description is depicted at right:

FHWA will include provisions in the contract to guarantee delivery services (FedEx, UPS, and USPS).

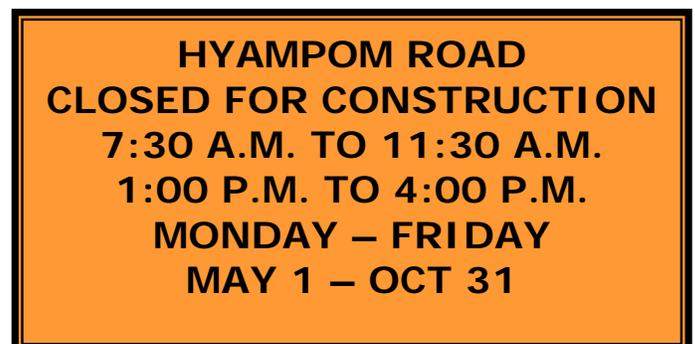


## Public Outreach

During construction, CFLHD will have a Public Information Program (PIP) in place with a designated public information officer (PIO) to administer it. The details of this plan are still being discussed but it would typically include a telephone hotline, web page (with links from local websites), signing, posters/pamphlets with latest information, local newspaper articles/ads, and coordination with services that use the roadway.



Daily Road closure Schedule	
7:30 to 11:30 am	Closed
11:30 am	Open westbound
11:45 am	Open eastbound
12:00 to 12:30 pm	Closed
12:30 pm	Open westbound
12:45 pm	Open eastbound
1:00 to 4:00 pm	Closed (except Thursdays)
4:00 to 7:00 pm	30 minute maximum delay, flaggers and/or pilot car
Thursday 1:00 to 4:00 pm	30 minute maximum delay, flaggers and/or pilot car
7:00 pm to 7:30 am weeknights and daytime on Saturdays	Occasional work with 30 minute maximum delay. Temporary traffic signal or stop/proceed when clear signs when no work.
Sundays, Federal Holidays and special events*	Open, no work, temporary traffic signal or stop/proceed when clear signs
Winter (November 1 to May 1)	Occasional work with 30 minute maximum delay. Temporary traffic signal or stop/proceed when clear signs when no work. Temporary pavement and erosion control devices in place.
*Special events include: Trinity County Fair, Hayfork High School Homecoming, Hyampom Good Times Fair, Hyampom Pie Auction, Fourth of July weekend. Citizens may request FHWA to provide openings for other specific special weekend events.	



## Timeline

FHWA will proceed to final design and will complete the design in early 2008. The project will then be advertised and awarded to a contractor in the spring of 2008 with construction anticipated to start after May 1<sup>st</sup>. FHWA will try to limit the two three- to four-hour closures for the first few months of the project while survey and clearing operations are taking place. During this time the contractor will work mostly with 30 minute maximum delays. The contractor also will mostly work with 30 minute maximum closures for the last few months of construction during paving and striping operations. The majority of construction will take place during the summer months (May to October) and is anticipated to last 3 construction seasons (fall of 2010). As FHWA presented at the special meeting in Hyampom, there are many variables and assumptions to this timeline. FHWA believes that in most cases the work can be completed in this time or less. FHWA will also evaluate bids based on the contractor's ability to finish the project in a timely manner.

For information and updates on this project, please visit the CFLHD Project webpage at [www.cflhd.gov/projects/](http://www.cflhd.gov/projects/) and click on California and then Hyampom Road.

If you have any questions, do not hesitate to contact us at:

Federal Highway Administration

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